




400 AIRWAYS AVENUE  
SAVANNAH, GA 31408

912.964.0514

TO: All Plan Holders  
Plan Rooms

FROM:   
Mark Denmark  
Acting Director of Engineering

DATE: June 4<sup>th</sup>, 2021

SUBJ: SAC 40238  
Runway 10-28 Paint Cleaning & Thermoplastic Sign Replacement  
Savannah Airport Commission  
Addendum No. 2

Attached please find Addendum No. 2 to the contract documents. All bidders shall acknowledge the receipt of Addendum No. 2 in the place provided in the bid proposal.

JAS  
ENCL: SAC 40238 – Addendum No. 2  
CC: Engineering Files  
Airfield Operations



## **SAVANNAH AIRPORT COMMISSION**

### **SAC 40238**

#### **Runway 10-28 Paint Cleaning & Thermoplastic Sign Replacement**

The following amendments, additions, deletions shall be made to the RFP documents. In so far as these documents are at variance with this Addendum No. 2 dated June 4th, 2021 the addendum shall govern:

- Plans & Specifications - Drawings

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# SAVANNAH / HILTON HEAD INTERNATIONAL AIRPORT

CONSTRUCTION  
PLANS FOR

## RUNWAY 10/28 PAINT CLEANING & THERMOPLASTIC SIGN REPLACEMENT



SAC NO.- 40328

ISSUED FOR  
BID

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SHEET 2. NOTES

SHEET 3. PAINT CLEANING PLAN AND  
QUANTITIES

SHEET 4. THERMOPLASTIC SIGN  
REPLACEMENT PLAN AND QUANTITIES



SAFETY

1. ENTRY SHALL BE BY GATE(S) DESIGNATED BY THE EXECUTIVE DIRECTOR OR HIS REPRESENTATIVE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR GATE SECURITY. NO PERSONAL VEHICLES OWNED BY CONTRACTORS' EMPLOYEES OR SUBCONTRACTORS SHALL BE ALLOWED ON THE AIRFIELD AT ANY TIME.
2. RADIO CONTACT WITH THE CONTROL TOWER MUST BE MAINTAINED BY ALL CONTRACTOR VEHICLES ON THE AIRFIELD. VEHICLES MUST CONTACT THE CONTROL TOWER UPON ENTERING ACTIVE RUNWAY, TAXIWAY, OR APRON AREA WHERE AIRCRAFT ARE MOVING OR ARE SUBJECT TO MOVE; AND IF WORKING WITHIN FIVE HUNDRED (500) FEET OF THE CENTERLINE OF ANY ACTIVE RUNWAY OR TWO HUNDRED (200) FEET OF THE CENTERLINE OF ANY (ACTIVE) TAXIWAY, THE CONTRACTOR SHALL MAINTAIN RADIO CONTACT WITH THE CONTROL TOWER AT ALL TIMES. IF THE CONTRACTOR HAS VEHICLES WITH NO RADIO, THEN SUCH VEHICLES SHALL FORM A CONVOY AND FOLLOW A VEHICLE HAVING TWO-WAY RADIO CONTACT WITH THE CONTROL TOWER. CONTRACTORS WORKING IN RUNWAY CLEAR ZONES SHALL MAINTAIN CONSTANT RADIO CONTACT WITH THE CONTROL TOWER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUPPLYING HIS OWN RADIOS.
3. THE CONTRACTOR SHALL REIMBURSE THE AIRPORT COMMISSION FOR THE FULL AMOUNT OF ANY FINES PLACED ON THE AIRPORT COMMISSION DUE TO AN UNAUTHORIZED CROSSING OF AN ACTIVE RUNWAY OR TAXIWAY BY THE CONTRACTOR OR ANY OF HIS SUBCONTRACTORS.
4. ALL VEHICLES, UPON ENTERING THE AIR OPERATIONS AREA SHALL DISPLAY AN ORANGE AND WHITE-CHECKERED FLAG, STAFF MOUNTED, OF NOT LESS THAN THREE (3) FEET SQUARE DISPLAYED ON THE VEHICLE. CRANES, BACKHOES, AND SIMILAR EQUIPMENT WORKING WITHIN FIVE HUNDRED (500) FEET OF THE CENTERLINE OR RUNWAYS AND TWO HUNDRED (200) FEET OF TAXIWAYS AND IN CLEAR ZONES, SHALL DISPLAY THE SAME SIZE AND TYPE OF FLAG SPECIFIED FOR VEHICLES ATTACHED TO THE BOOM. CRANE BOOMS SHALL BE LOWERED WHEN NOT IN USE.
5. MATERIAL OR PERSONAL VEHICULAR STORAGE AREA SHALL BE ASSIGNED BY THE EXECUTIVE DIRECTOR OR HIS REPRESENTATIVE.
6. MATERIALS TO BE STORED ON AIRPORT PROPERTY SHALL NOT CREATE AN OBSTRUCTION TO AIR NAVIGATION NOR SHALL THEY INTERFERE WITH THE FREE UNOBSTRUCTED MOVEMENT OF AIRCRAFT.
7. LOOSE MATERIALS CAPABLE OF CAUSING DAMAGE TO AIRCRAFT LANDING GEARS, PROPELLERS, OR BEING INGESTED IN JET ENGINES, SHALL NOT BE STORED ON OR AROUND ACTIVE AIRCRAFT MOVEMENT AREAS.
8. STOCKPILED MATERIAL WILL BE CONSTRAINED IN A MANNER TO PREVENT MOVEMENT RESULTING FROM AIRCRAFT BLAST OR WIND CONDITIONS IN EXCESS OF 10 KNOTS. STOCKPILED MATERIAL SHALL BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING YELLOW LIGHTS DURING HOURS OF RESTRICTED VISIBILITY.
9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SECURITY OF HIS MATERIAL AND EQUIPMENT.
10. ALL OPEN TRENCHES, EXCAVATIONS WITHIN THE AIR OPERATIONS AREA, SHALL BE MARKED BY LIGHTED AND FLAGGED BARRICADES. BARRICADES SHALL BE ALTERNATE ORANGE AND WHITE MARKINGS WITH FLASHING YELLOW LIGHTS AND A MAXIMUM OF 18 INCHES IN HEIGHT. BARRICADES ADJACENT TO RUNWAYS OR TAXIWAY PAVEMENT AREAS SHALL BE REQUIRED TO BE SECURED IN SUCH MANNER TO PREVENT TIPPING OVER. FLAGS SHALL BE ORANGE AND WHITE, STAFF MOUNTED, AND NOT LESS THAN 20" X 20". ALL BARRICADES SHALL BE SUBJECT TO APPROVAL BY THE EXECUTIVE DIRECTOR. THE CONTRACTOR SHALL PROVIDE THE NAME AND PHONE NUMBER OF TWO INDIVIDUALS TO BE ON CALL 24 HOURS PER DAY FOR EMERGENCY MAINTENANCE OF BARRICADE LIGHTING.
11. ALL CONSTRUCTION WORK CLOSER THAN ONE HUNDRED TWENTY-FIVE (125) FEET OF THE EDGE OF A RUNWAY OR EIGHTY-FIVE (85) FEET FROM THE EDGE OF A TAXIWAY WILL REQUIRE TEMPORARY CLOSING OF THE RUNWAY OR TAXIWAY. TEMPORARILY CLOSED TAXIWAYS SHALL BE MARKED BY LIGHTED AND WEIGHTED BARRICADES AS SHOWN ON THE PLANS. TEMPORARILY CLOSED RUNWAYS SHALL BE MARKED WITH A CROSS PLACED ON THE RUNWAY NUMBERS BY THE SAVANNAH AIRPORT COMMISSION.
12. PRIOR TO BEGINNING ANY EXCAVATION WITHIN TWO HUNDRED (200) FEET OF THE CENTERLINE OF ANY RUNWAY OR TAXIWAY, THE CONTRACTOR SHALL NOTIFY THE EXECUTIVE DIRECTOR OR HIS REPRESENTATIVE. ALL TRENCH EXCAVATION WITHIN THE AIR OPERATIONS AREA SHALL BE BACKFILLED AND COMPACTED AT THE END OF EACH WORKDAY.
13. CONSTRUCTION EQUIPMENT OR MATERIAL SHALL NOT BE STORED WITHIN THE AIR OPERATIONS AREA DURING HOURS OF RESTRICTED VISIBILITY OR DARKNESS WITHOUT THE APPROVAL OF THE EXECUTIVE DIRECTOR OR HIS REPRESENTATIVE.
14. OPEN FLAME WELDING OR TORCH CUTTING OPERATIONS ARE PROHIBITED UNLESS FIRE AND SAFETY PRECAUTIONS ARE PROVIDED IN ACCORDANCE WITH NFPA CODES AND APPROVED BY THE OWNER. OPEN FLAME WELDING OR TORCH CUTTING WILL BE PERMITTED ON AIRPORT PROPERTY BY OBTAINING A PERMIT FROM THE AIRPORT FIRE DEPARTMENT.
15. ANY USE OF OXYGEN/ACETYLENE WELDING EQUIPMENT OR OPEN FLAME EQUIPMENT
16. SHALL REQUIRE THE CONTRACTOR TO OBTAIN A PERMIT FROM THE AIRPORT FIRE CHIEF.
17. VEHICULAR TRAFFIC CROSSING ACTIVE AIRCRAFT MOVEMENT AREAS (RUNWAYS, TAXIWAYS OR AIRCRAFT PARKING APRONS) SHALL BE CONTROLLED EITHER BY TWO-WAY RADIO CONTACT WITH THE CONTROL TOWER, BY ESCORT, FLAGMAN, SIGNAL LIGHTS, OR OTHER APPROPRIATE MEANS AS APPROVED BY THE FAA CONTROL TOWER CHIEF. AFTER RECEIVING CLEARANCE FROM THE CONTROL TOWER, THE DRIVER'S PERSONAL OBSERVATION THAT NO AIRCRAFT IS APPROACHING HIS POSITION WILL BE MADE BEFORE HE MAKES ANY CROSSING OF ACTIVE TAXIWAY OR RUNWAY. **THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUPPLYING HIS OWN RADIOS.**
18. IF IT IS DESIRABLE TO CLEARLY IDENTIFY THE VEHICLES FOR CONTROL PURPOSES BY EITHER ASSIGNED INITIALS OR NUMBERS, THEN THE IDENTIFYING SYMBOL SHALL BE OF EIGHT (8) INCH MINIMUM, BLOCK-STYLE CHARACTER OF A COLOR EASILY READ. SYMBOLS MAY BE APPLIED BY USE OF TAPE OR WATER-SOLUBLE PAINT.
19. MOTORIZED VEHICLES AND EQUIPMENT OPERATING IN THE AOA SHALL NOT

EXCEED FIFTEEN (15) MILES PER HOUR.

20. AIRCRAFT SHALL HAVE PRIORITY OVER ALL MOTORIZED VEHICLES AND EQUIPMENT.

21. ALL CONSTRUCTION DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY IN ACCORDANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS. ALL CLEAN FILL MATERIAL SHALL REMAIN THE PROPERTY OF THE SAVANNAH AIRPORT COMMISSION AND BE DISPOSED OF ON AIRPORT PROPERTY AS DIRECTED BY THE SAVANNAH AIRPORT COMMISSION.

22. CONSTRUCTION NOTAMS SHALL BE ISSUED BY THE EXECUTIVE DIRECTOR OR HIS REPRESENTATIVE. CONSTRUCTION CAUSING RUNWAY OR TAXIWAY CLOSURES SHALL BE KEPT TO A MINIMUM AND SCHEDULED CLOSURES SHALL BE DISCUSSED WITH THE EXECUTIVE DIRECTOR OR HIS REPRESENTATIVE AS FAR IN ADVANCE AS POSSIBLE, BUT NOT LESS THAN FORTY-EIGHT (48) HOURS IN ADVANCE. LANDING AND TAKING OFF OF SCHEDULED AIRLINES SHALL HAVE PRIORITY.

23. CONTRACTOR(S) SHALL CONSIDER PERMANENT MEANS OF CONTROL OR PREVENTION OF SOIL EROSION NOT ONLY TO PRESERVE AND PROTECT THE SLOPES, PAVEMENT AND OTHER FACILITIES, BUT ALSO TO REDUCE POTENTIAL SOURCES OF WATER POLLUTION.

24. ALL ELECTRICAL AND CONTROL CABLES SHALL BE BURIED A MINIMUM OF THIRTY-SIX (36) INCHES BELOW THE SURFACE OF THE GROUND.

SECURITY

1. CONTRACTOR SHALL BE RESPONSIBLE FOR THE SECURITY OF HIS EQUIPMENT AND MATERIALS. HE SHALL BE RESPONSIBLE FOR THE SECURITY OF ALL PERIMETER SECURITY GATES, TERMINAL DOORS AND HATCHES LEADING TO SECURE AREAS UTILIZED BY HIM. AS DIRECTED BY THE EXECUTIVE DIRECTOR, LOCKS SHALL BE PLACED ON EACH GATE USED BY THE CONTRACTOR. THE LOCKS MUST BE MARKED IN A MANNER SHOWING COMPANY OWNERSHIP AND A KEY OR COMBINATION PROVIDED TO THE AIRPORT PUBLIC SAFETY DEPARTMENT. THE GATES SHALL BE LOCKED AT ALL TIMES OR GUARDS POSTED AT THE GATES TO CONTROL ACCESS THROUGH THEM. GATE GUARDS SHALL HAVE A RADIO OR CELLULAR PHONE WHICH WILL ENABLE THEM TO CALL THE POLICE TO REPORT SECURITY PROBLEMS OR THE CONTRACTOR TO VERIFY IDENTITIES, ETC. FOR JOINT USE GATES, IF A LOCK IS FOUND UNSECURED, THE COMPANY OWNING THE LOCK IS IN VIOLATION OF AIRPORT RULES AND REGULATIONS. IN ADDITION, UNAUTHORIZED ENTRY TO THE AIR OPERATIONS AREA THROUGH THE GATES MAY RESULT IN THE RESPONSIBLE PARTY BEING CITED FOR VIOLATING AIRPORT REGULATIONS.

2. THE TRANSPORTATION SECURITY ADMINISTRATION ACT 2002, 49 USC, 67FR8355, GIVES THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) AUTHORITY TO PLACE A FINE ON ANY AIRPORT FOUND TO BE IN BREACH OF A SECURITY REQUIREMENT.

3. THE CONTRACTOR SHALL REIMBURSE THE AIRPORT COMMISSION FOR THE FULL AMOUNT OF ANY FINES PLACED ON THE AIRPORT COMMISSION DUE TO NEGLIGENCE ON THE PART OF THE CONTRACTOR. FINES MAY BE PLACED ON THE AIRPORT COMMISSION FOR SUCH THINGS AS SECURITY GATES BEING UNLOCKED, TERMINAL DOORS NOT SECURE, FENCES TORN DOWN, AND AIR OPERATIONS AREA NOT BEING PROPERLY SECURED. THESE ARE ONLY EXAMPLES OF ITEMS CAUSING FINES AND NOT LIMITATIONS. THERE COULD BE OTHER RELATED ITEMS.

4. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PREVENT ANY BREACH OF SECURITY WITHIN HIS AREA OF CONSTRUCTION OR ANY ROUTE OF ENTRY TO AREA OF CONSTRUCTION.

5. ALL PERSONNEL HAVING UNESCORTED ACCESS TO ANY SECURITY RESTRICTED AREA SHALL WEAR VALID SAVANNAH INTERNATIONAL AIRPORT IDENTIFICATION BADGES SO THEY ARE VISIBLE ON THEIR OUTER GARMENTS IN SUCH AREAS AT ALL TIMES TO PERMIT READY RECOGNITION BY AIRPORT PUBLIC SAFETY OFFICERS. CONTRACTORS' EMPLOYEES MAY BE ISSUED ANY ONE OF THE BELOW LISTED SECURITY IDENTIFICATION, ETC. BADGES.

- a. AIRPORT IDENTIFICATION BADGES ARE ISSUED TO APPROVED PERSONNEL IN SEVERAL COLORS:

1. BLUE - ISSUED TO PERSONNEL REQUIRING UNLIMITED ACCESS INSIDE THE SECURED SIDA.

(1)EFFECTIVE DECEMBER 6, 2002, THE TSA REQUIRES ANYONE REQUESTING UNESCORTED ACCESS TO THE SECURED SIDA SHALL BE FINGERPRINTED, A BACKGROUND CHECK PERFORMED, AND RESULTS RETURNED PRIOR TO ID BADGE BEING ISSUED. **NO EXCEPTIONS.** THIS PROCESS TAKES 3-14 DAYS. ANYONE APPLYING FOR BADGES SHALL SUBMIT APPLICATION AS SOON AS POSSIBLE TO ENSURE FINGERPRINTS / CRIMINAL HISTORY RECORDS ARE RETURNED PRIOR TO START DATE OF PROJECT.  
(2)THE COST FOR PROCESSING IS \$30.00 FOR FINGERPRINTING AND \$25.00 PER BADGE, PER PERSON. EVERYONE RECEIVING A SIDA ID BADGE MUST BE FINGERPRINTED.

2. YELLOW - ISSUED TO CONTRACTORS WORKING IN THE VICINITY OF THE AIRCRAFT MOVEMENT AREA IN ORDER TO PERFORM THEIR REQUIRED DUTIES. **PERSONS WITH YELLOW BADGES MAY NOT ENTER THE SECURED SIDA.**

3. GREY - ISSUED TO GENERAL AVIATION AND TENANTS WHO REQUIRE INCIDENTAL ACCESS TO THE 1542.203 AREAS. **PERSONS WITH PINK OR GREY BADGES MAY NOT ENTER THE SECURED SIDA.**

- b. THE COLOR OF THE BADGE SIGNIFIES THE AREA ON THE AIRPORT WHERE THE BADGE HOLDER MAY OPERATE.

1. IDENTIFICATION BADGES MUST BE CONTROLLED AT ALL TIMES. WHEN PERSONNEL ARE TERMINATED, UPON COMPLETION OF THE CONSTRUCTION PROJECT, AND WHEN BADGES EXPIRE, THE CONTRACTOR IS RESPONSIBLE FOR RETURNING IDENTIFICATION BADGES TO THE AIRPORT PUBLIC SAFETY DEPARTMENT. BEFORE A NEW BADGE IS ISSUED TO ANY PERSON, THEIR EXPIRED OR INVALID BADGE MUST BE RETURNED TO THE AIRPORT PUBLIC SAFETY DEPARTMENT.

UPON COMPLETION OF A PROJECT, IT WILL BE THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO COLLECT ALL BADGES ISSUED UNDER HIS CONTRACT. SUBCONTRACTORS ARE RESPONSIBLE FOR COLLECTING THEIR BADGES. BEFORE FINAL PAYMENT IS MADE ON THE PROJECT, A WRITTEN NOTIFICATION FROM THE AIRPORT PUBLIC SAFETY DEPARTMENT WILL BE GIVEN TO THE DIRECTOR OF ENGINEERING. THE WRITTEN NOTICE WILL STATE THE NUMBER OF BADGES ISSUED AND THE NUMBER OF BADGES RETURNED.

2. A FEE OF \$20.00 (WITHOUT READER), \$25.00 (WITH READER), PAYABLE IN ADVANCE, IS CHARGED FOR EACH BADGE ISSUED. EACH CONTRACTOR AND SUBCONTRACTOR SHALL MAKE A CASH DEPOSIT OF \$200 PRIOR TO RECEIVING ANY BADGES. THIS DEPOSIT IS REFUNDABLE PROVIDING ALL BADGES HAVE BEEN RETURNED. FOR EACH BADGE NOT RETURNED BY THE CONTRACTOR OR SUBCONTRACTOR, \$200.00 WILL BE DEDUCTED FROM ANY MONIES DUE THE CONTRACTOR OR HIS SURETY. ALL COSTS, I.E., ID BADGE, FINGERPRINT REQUIREMENTS, AND DEPOSIT(S) SHALL BE PAID IN ADVANCE.

3. THE CONTRACTOR SHALL BE REQUIRED TO COMPLY WITH THE TRANSPORTATION SECURITY ADMINISTRATION AMENDMENT TO PART 1542.209 PRIOR TO COMMENCING WORK. ALL PERSONNEL HIRED AFTER DECEMBER 6, 2002, WHO HAVE UNESCORTED ACCESS TO ANY AREA ON THE AIRPORT CONTROLLED FOR SECURITY REASONS SHALL HAVE BACKGROUND CHECKS TO THE EXTENT ALLOWABLE BY LAW, INCLUDING AT A MINIMUM, REFERENCES AND PRIOR EMPLOYMENT HISTORIES TO THE EXTENT NECESSARY TO VERIFY REPRESENTATIONS MADE BY THE EMPLOYEE/APPLICANT RELATIVE TO EMPLOYMENT IN THE PRECEDING TEN (10) YEARS. THE CONTRACTOR SHALL CERTIFY TO THE COMMISSION BY USING SAC FORM 513 THAT SUCH CHECKS WERE CONDUCTED AND ARE ON FILE IN THE CONTRACTOR'S OFFICE FOR INSPECTION BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA) OR SAVANNAH AIRPORT COMMISSION REPRESENTATIVES.



SAVANNAH AIRPORT COMMISSION  
Engineering & Construction

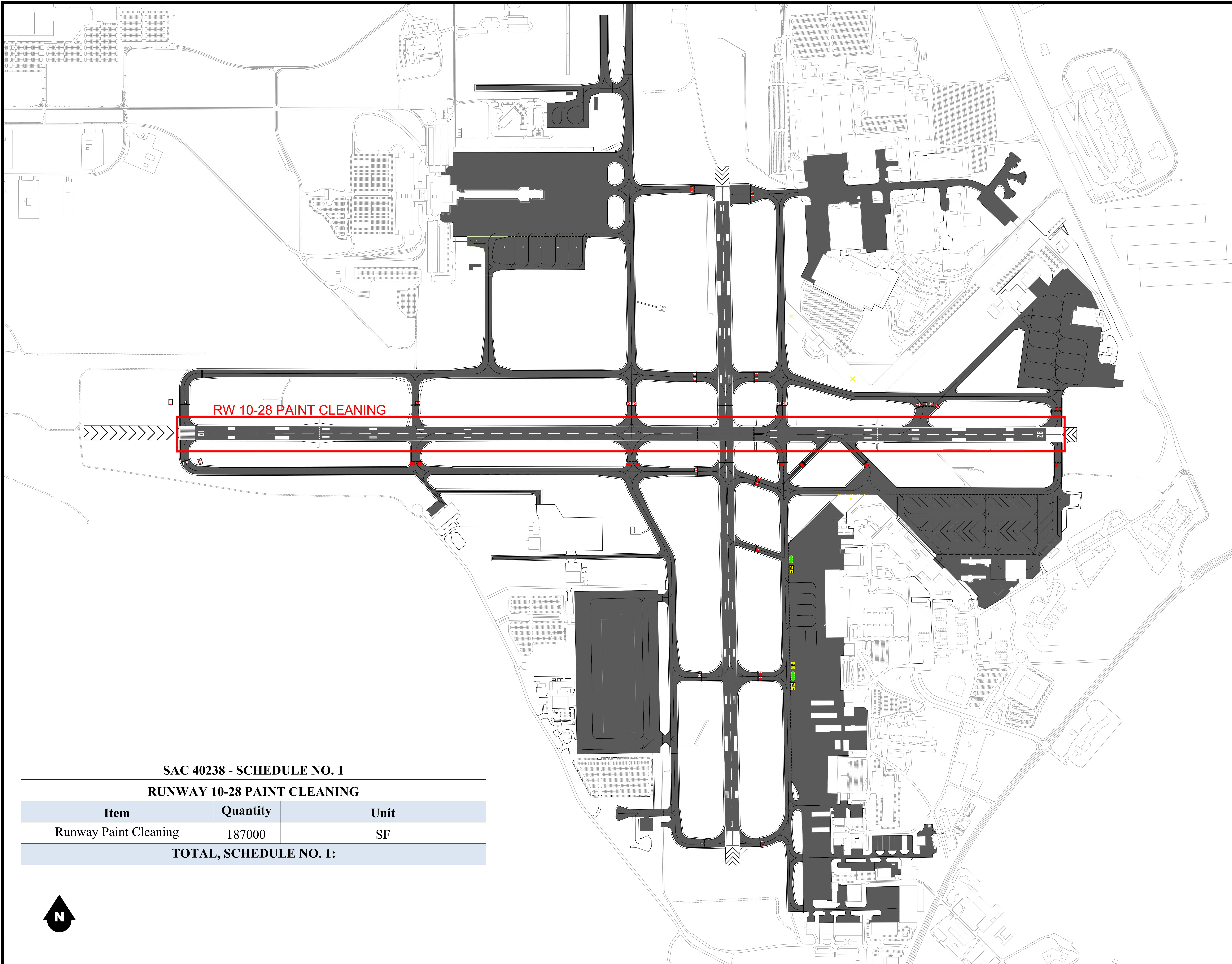
RUNWAY 10/28 PAINT CLEANING &  
THERMOPLASTIC SIGN REPLACEMENT

PROJ NO: 40328
SCALE: 1"=50'
DATE: 5/11/2021
DESIGNED BY:
DRAWN BY: JTE
CHECKED BY:

SHEET TITLE:

NOTES

SHEET NO:

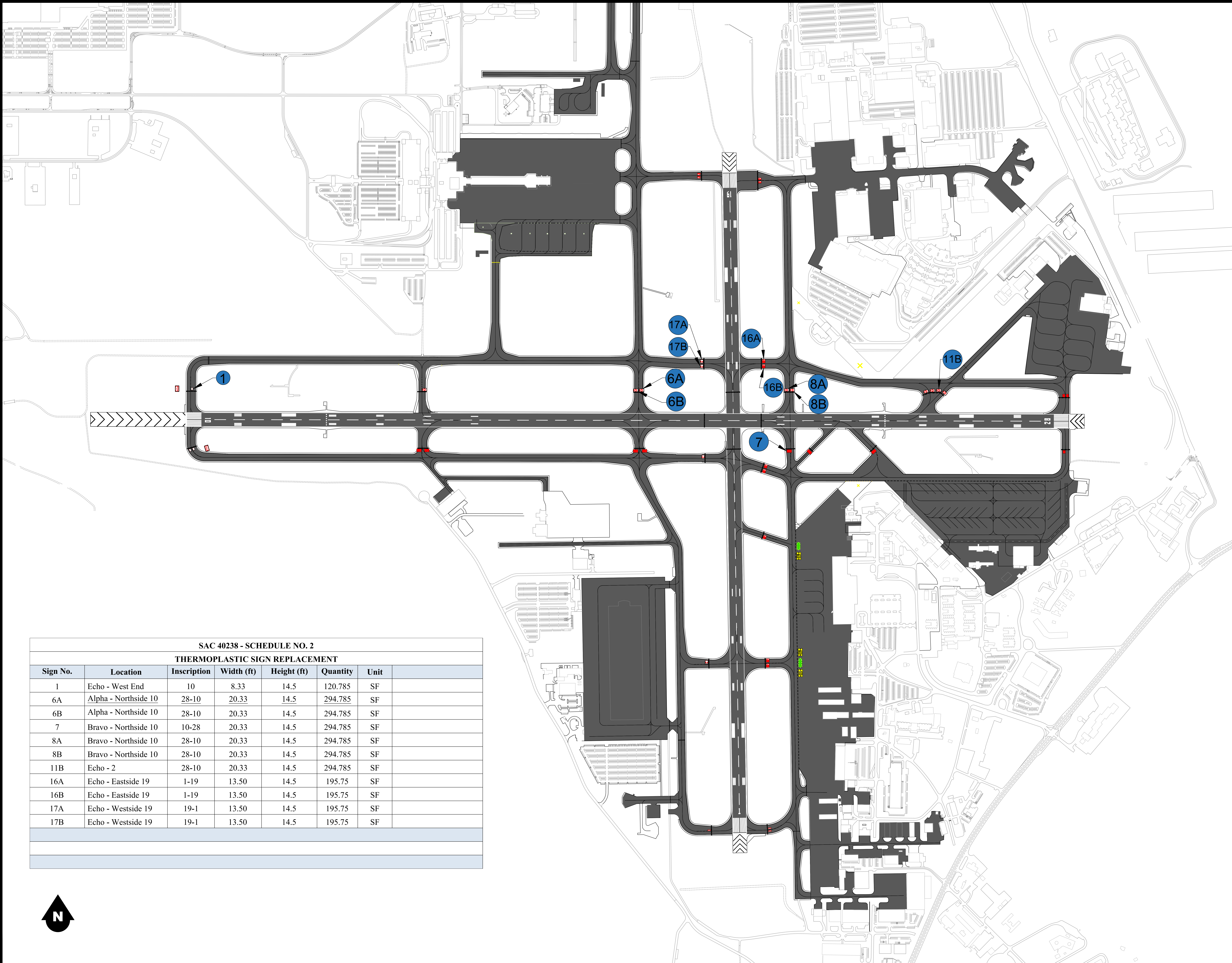


RW 10-28 PAINT CLEANING

SAC 40238 - SCHEDULE NO. 1

RUNWAY 10-28 PAINT CLEANING

Item	Quantity	Unit
Runway Paint Cleaning	187000	SF
TOTAL, SCHEDULE NO. 1:		



SAC 40238 - SCHEDULE NO. 2						
THERMOPLASTIC SIGN REPLACEMENT						
Sign No.	Location	Inscription	Width (ft)	Height (ft)	Quantity	Unit
1	Echo - West End	10	8.33	14.5	120.785	SF
6A	Alpha - Northside 10	28-10	20.33	14.5	294.785	SF
6B	Alpha - Northside 10	28-10	20.33	14.5	294.785	SF
7	Bravo - Northside 10	10-28	20.33	14.5	294.785	SF
8A	Bravo - Northside 10	28-10	20.33	14.5	294.785	SF
8B	Bravo - Northside 10	28-10	20.33	14.5	294.785	SF
11B	Echo - 2	28-10	20.33	14.5	294.785	SF
16A	Echo - Eastside 19	1-19	13.50	14.5	195.75	SF
16B	Echo - Eastside 19	1-19	13.50	14.5	195.75	SF
17A	Echo - Westside 19	19-1	13.50	14.5	195.75	SF
17B	Echo - Westside 19	19-1	13.50	14.5	195.75	SF